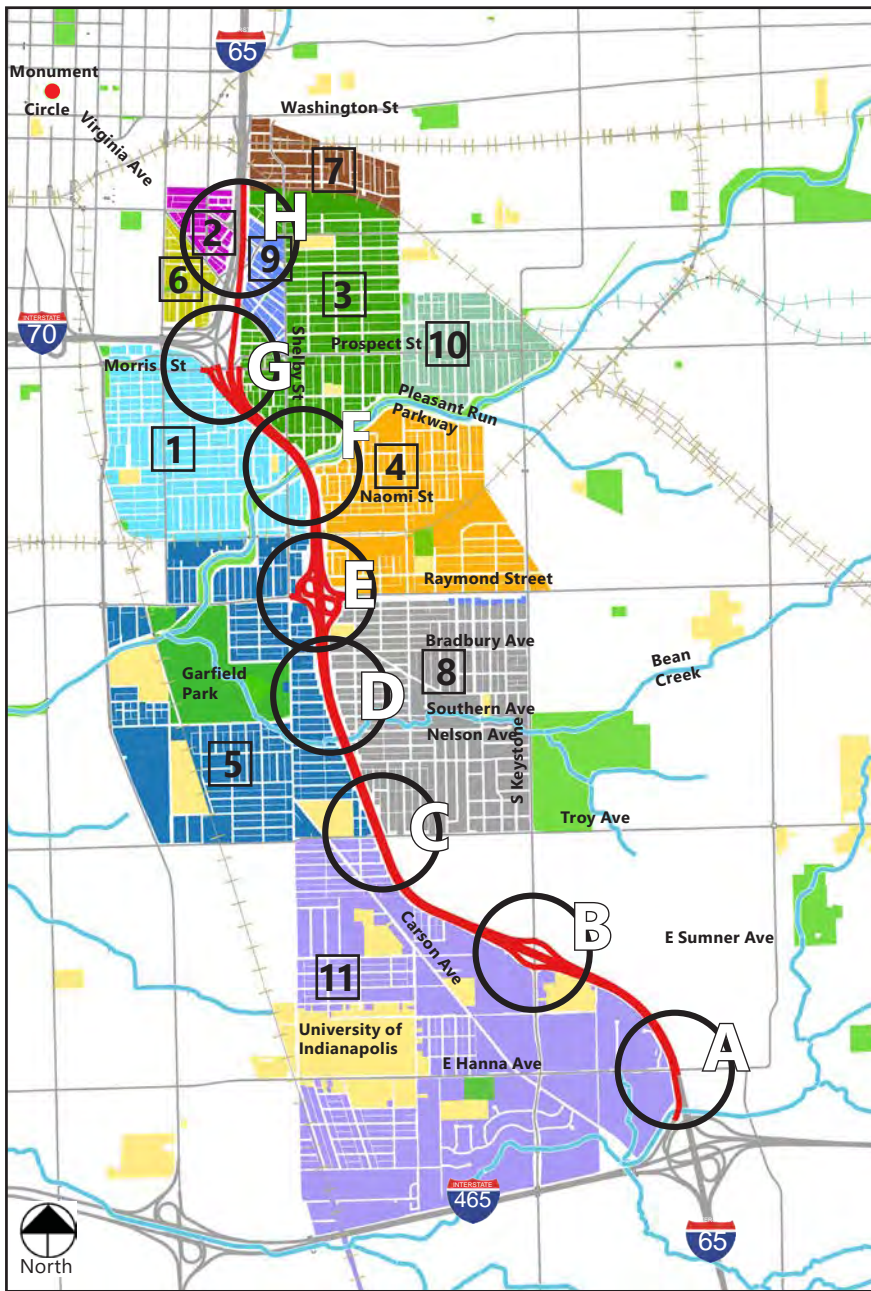


Section 3: Study Areas



NEIGHBORHOODS

- 1** Bates-Hendricks
- 2** Fletcher Place
- 3** Fountain Square
- 4** Fountain Sq. Alliance
- 5** Garfield Park
- 6** Holy Rosary
- 7** Irish Hill
- 8** Bean Creek
- 9** North Square
- 10** Prospect Falls
- 11** University Heights

- Indy Parks
- Schools
- Rethin Coalition I-65 SAFE Project Study Areas

SECTION

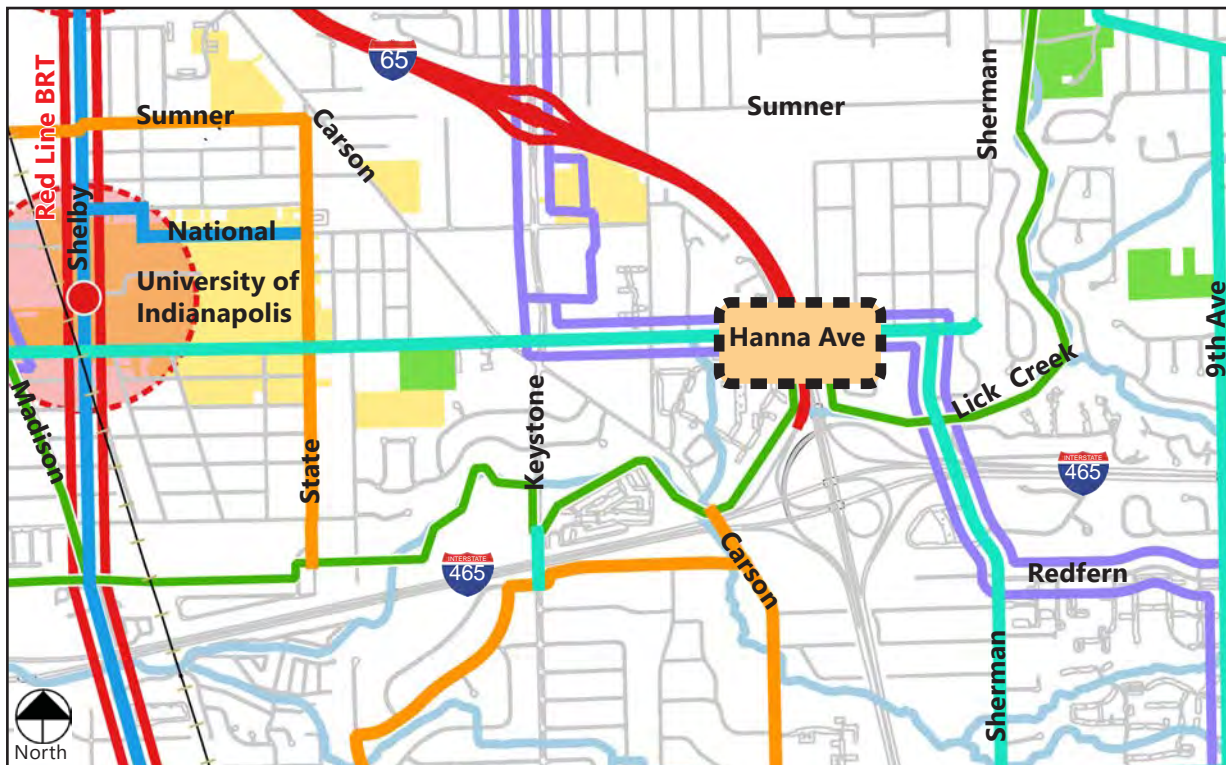
PAGE

SECTION 3: STUDY AREAS		3.1
A	Hanna Avenue Bridge	3.2
B	Keystone Avenue Interchange	3.6
C	Troy Avenue Bridge/Underpass	3.10
D	Nelson/Southern/Bradbury	3.14
E	Raymond Street Interchange	3.17
F	CSX/Naomi/Pleasant Run/Shelby Underpasses	3.21
G	Morris/Prospect Street Interchange and Bridges	3.28

Study Area A: Hanna Ave Bridge



Study Area A: Hanna Avenue Bridge/Context



HANNA AVE PORTAL STUDY AREA

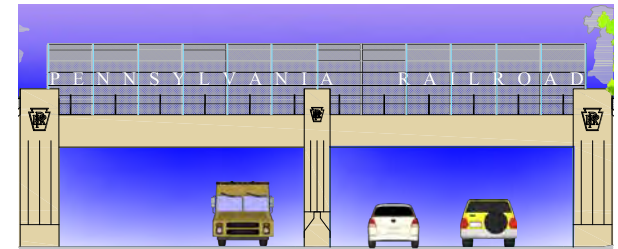
- | | | | |
|---|----------------|------------------------------|--|
| Existing & Planned Bike/Pedestrian System | | IndyGo Public Transit System | |
| | Greenway | | Local Transit Route |
| | Multi Use Path | | Red Line BRT Stop
1/4 Mile R Walkshed |
| | Neighborway | | |
| | Bike Lane | | |

Rethink Coalition Observations

Hanna Ave is designated as a "Complete Street" for the portion over I-65 as shown on the *Indy Moves* plan which includes bicycle/pedestrian facilities, ideally on both sides of the street. Since the I-65 SAFE project will replace the bridge, Rethink Coalition recommends that it comply with recommended practice to include a safety barrier rail between vehicular traffic and counter-flow cyclist traffic on both sides with other considerations. The new usage pattern for this overpass is reason to consider the safety fence indicated on the recommended cross-section to protect interstate motorists.

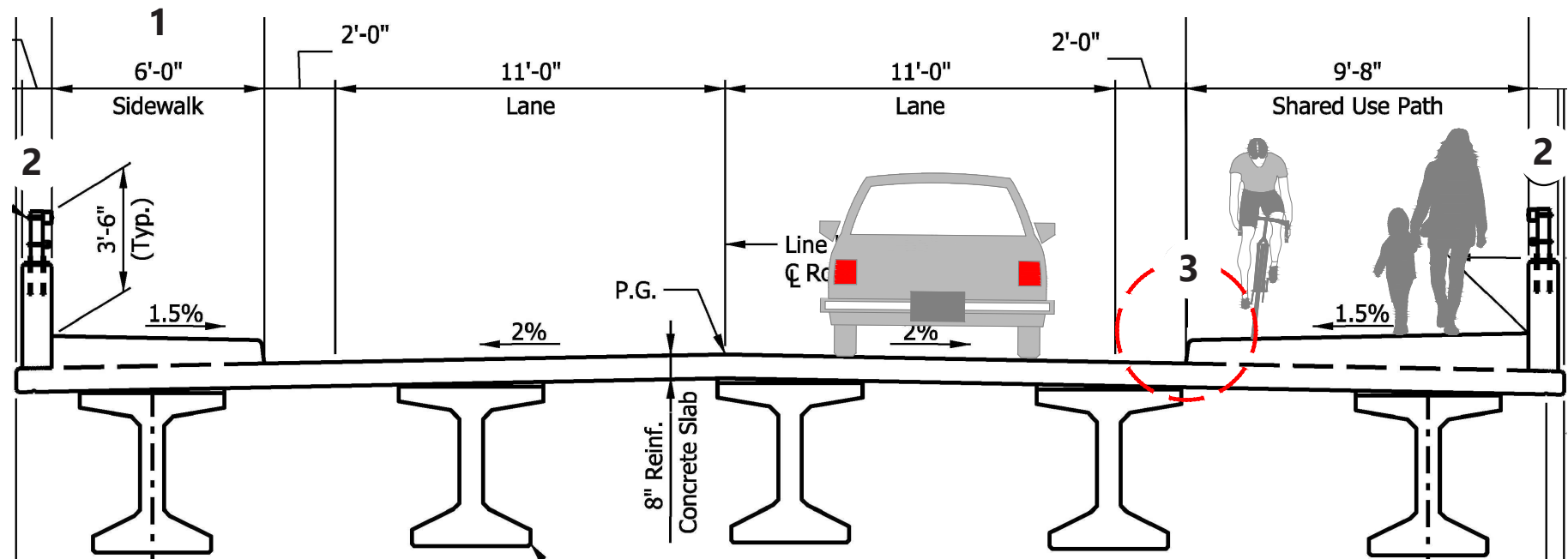


Existing Hanna Avenue Bridge over I-65



An example of a naming opportunity from the Pennsy Trail at the Shadeland Avenue overpass. Substitute "Lick Creek Greenway".

Study Area A: Hanna Avenue Bridge/INDOT Cross Section

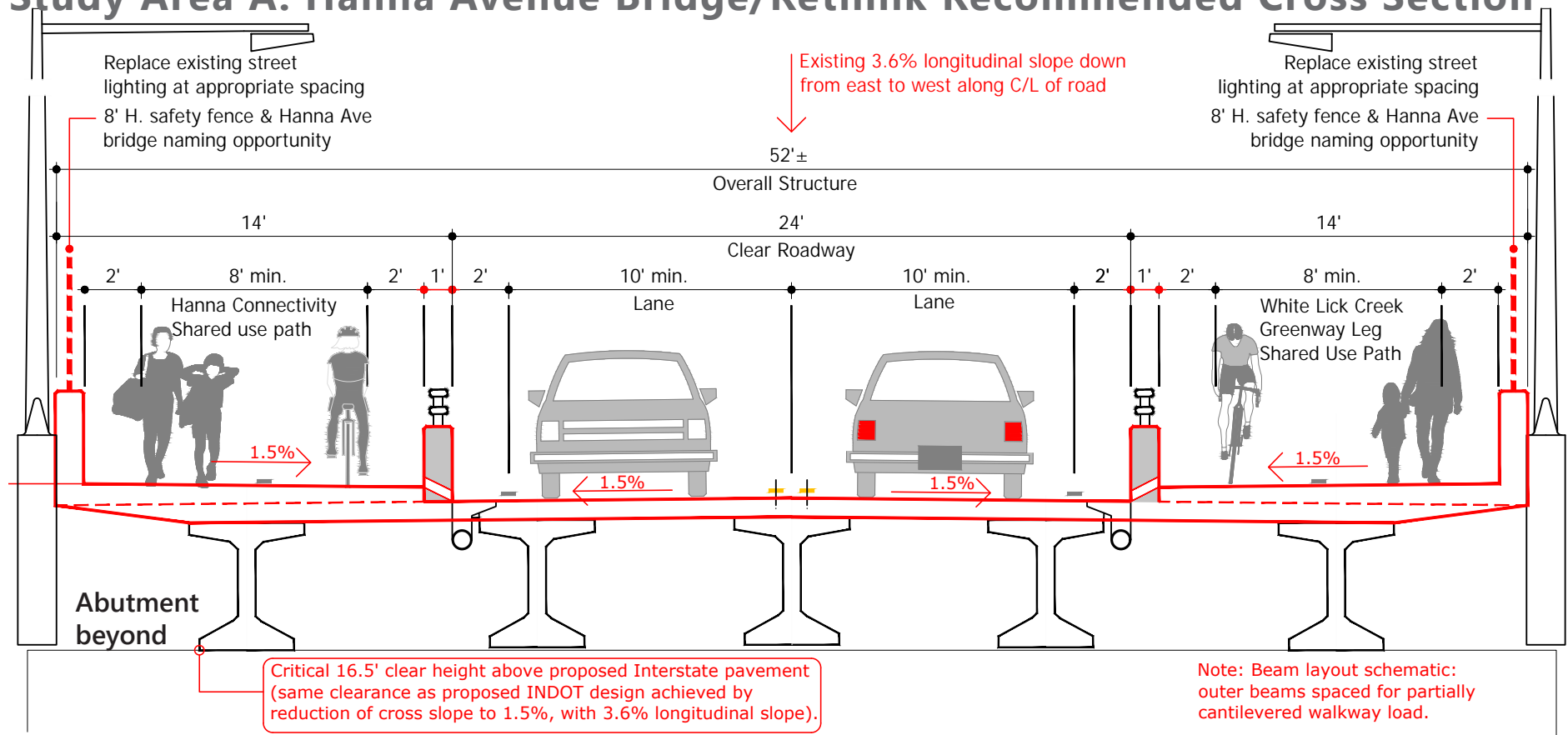


INDOT PROPOSAL & RETHINK OBSERVATIONS

The proposed Complete Streets improvements to the new Hanna Avenue Bridge are not fully compliant with bicycle/pedestrian path safety standards and do not address the usage distinction between a Lick Creek Greenway leg and Hannah Avenue connectivity. Those combined new uses also create an unaddressed interstate safety hazard:

- 1 Six-foot sidewalk [NIC curb] does not meet city standards, is unsafe for potential cyclist usage that will be generated here.
- 2 Increased usage generated by two planned greenways warrants safety fence installation for protection.
- 3 Insufficient space for shared use path and the safety rail required for protection from counterflow vehicular traffic.

Study Area A: Hanna Avenue Bridge/Rethink Recommended Cross Section

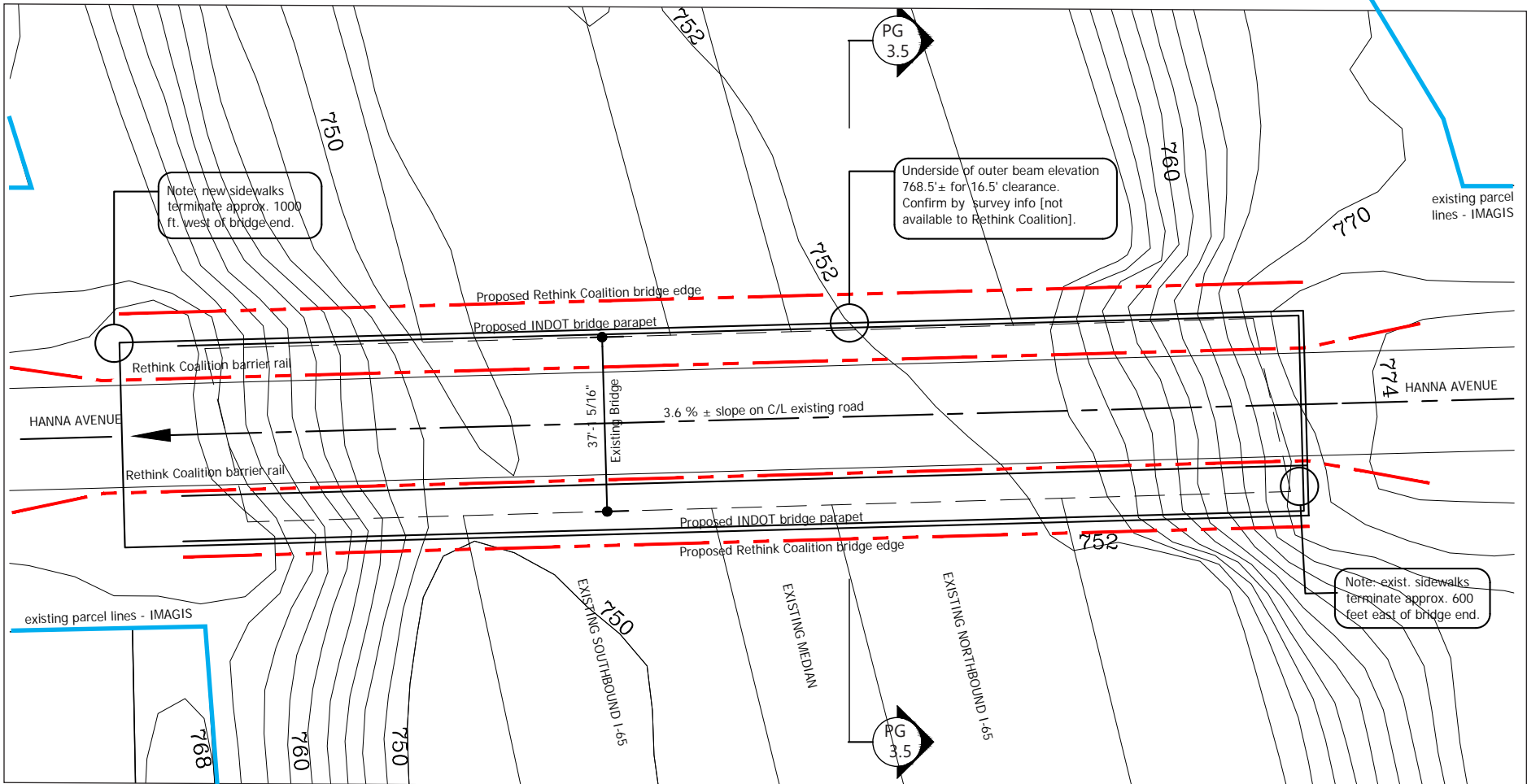


RETHINK RECOMMENDED CROSS SECTION

Provide a shared use path on both sides to support planned sidewalk development on Hanna as well as the Lick Creek Greenway on the south side of the bridge. The indicated widening of the bridge could be reduced by up to four feet if bicycle use on the north side is considered incidental. Recommended improvements include:

- Eight-foot wide minimum path width plus two-foot shy zone buffers from railings.
- Safety railing protection of cyclists from counterflow vehicular traffic on both sides.
- Safety fence to protect interstate vehicles from objects thrown over bridge railings.
- Bridge lighting at each approach lane to define narrowing of roadway.
- An additional opportunity is to name the Hanna Ave/Lick Creek Greenway on the Safety Fences per the Pennsy Trail example over Shadeland Avenue.

Study Area A: Hanna Avenue Bridge/Rethink Recommended Plan



Central Bridge barrier rail along curb line showing end treatment. End treatment same at both ends and symmetrical both sides. If sufficient space is available the end treatment could also flare away from the roadway as shown in the diagram above. Speed limit on Central Avenue is 35 mph, same as Hanna Avenue.

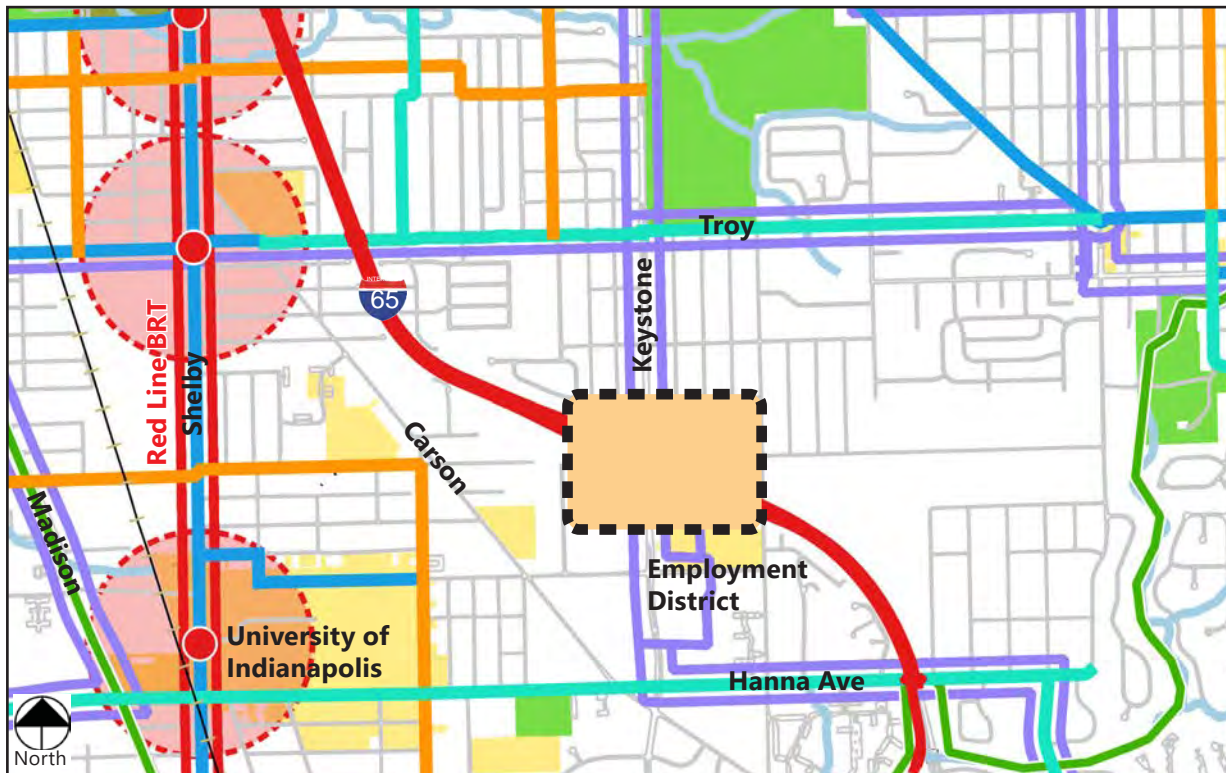


EB Hanna Bridge (Existing)

Study Area B: Keystone Avenue Interchange



Study Area B: Keystone Avenue Interchange/Context



**KEYSTONE AVE
PORTAL STUDY AREA**

- | | |
|--|---------------------------------|
| Existing & Planned
Bike/Pedestrian System | IndyGo Public Transit
System |
| Greenway | Local Transit
Route |
| Multi Use Path | Red Line BRT Stop |
| Neighborway | 1/4 Mile R Walkshed |
| Bike Lane | |

Rethink Coalition Observations

Keystone Ave is designated as a 4-lane Primary Arterial in the *Indianapolis Thoroughfare Plan*. The *Indy Moves* plan does not designate any "Pedal Indy" facilities.

The commercial businesses on the west and south sides of the interchange are employment destinations, with dense residential areas north of the bridge.

The new Keystone Bridge seems to work well for vehicular traffic but has poor accommodation for bikes and pedestrians through the underpass, even though the commercial zone is an employment destination.

Rethink Coalition recommends funding from the I-65 SAFE project be utilized to remedy the deficient bicycle/pedestrian facilities through the interchange.

The recent but incomplete work for pedestrian accommodation at Raymond Street could be a model for improved facilities at Keystone: short crosswalk distances, stacking room for a vehicle when a pedestrian is in the crosswalk, and continuous pedestrian travel path.

Study Area B: Keystone Avenue Interchange/Context



ENLARGEMENT AERIAL FOR KEYSTONE AVE AT I-65

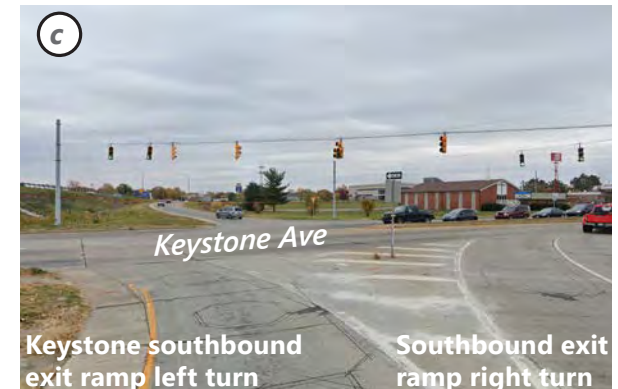
Study Location B: Keystone Avenue Observations & Recommendations



75-foot unprotected oblique angle pedestrian crossing incompatible with high speed turning lane to the I-65 ramp



87-foot unprotected pedestrian crossing conflicts with high speed turning lanes.



75-foot distance across turning lanes with permissive right turn, no physical refuge

Pedestrian Crossing Observations

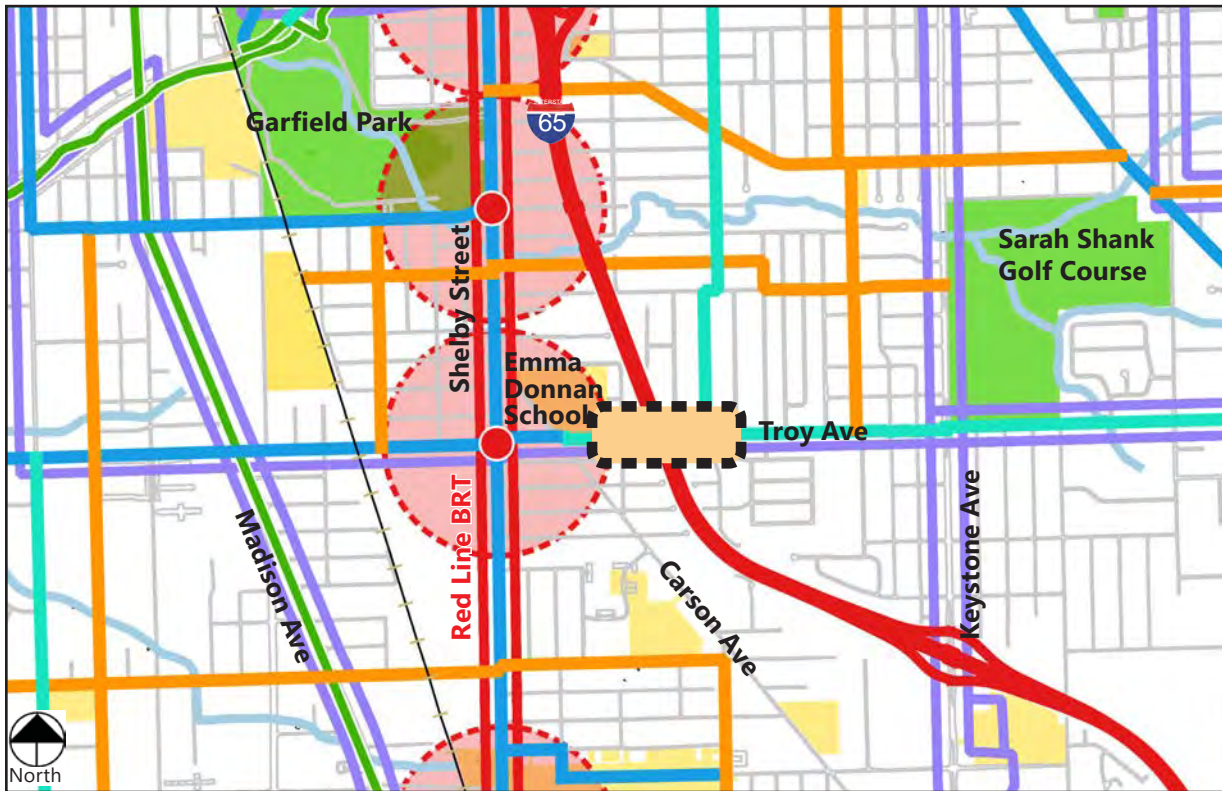
Pedestrians [without disability] need 18-21 seconds at 3.5 to 4 feet per second to cross this 75 foot crossing. Vehicle stopping distance at 40 mph ranges from 80-120 feet once the driver reacts. A significant pedestrian hazard.

It is difficult for a pedestrians or cyclists to safely navigate through any of the free-flowing turning lanes at this interchange which separates residential areas from an employment district.

Rethink Recommendation

Implement uniform bicycle pedestrian crosswalk safety measures throughout the interchange based on an intersection study.

Study Area C: Troy Avenue Bridge Context



TROY AVENUE UNDERPASS PORTAL STUDY AREA

- | | |
|---|------------------------------|
| Existing & Planned Bike/Pedestrian System | IndyGo Public Transit System |
| Greenway | Local Transit Route |
| Multi Use Path | Red Line BRT Stop |
| Neighborway | 1/4 Mile R Walkshed |
| Bike Lane | |

Observations

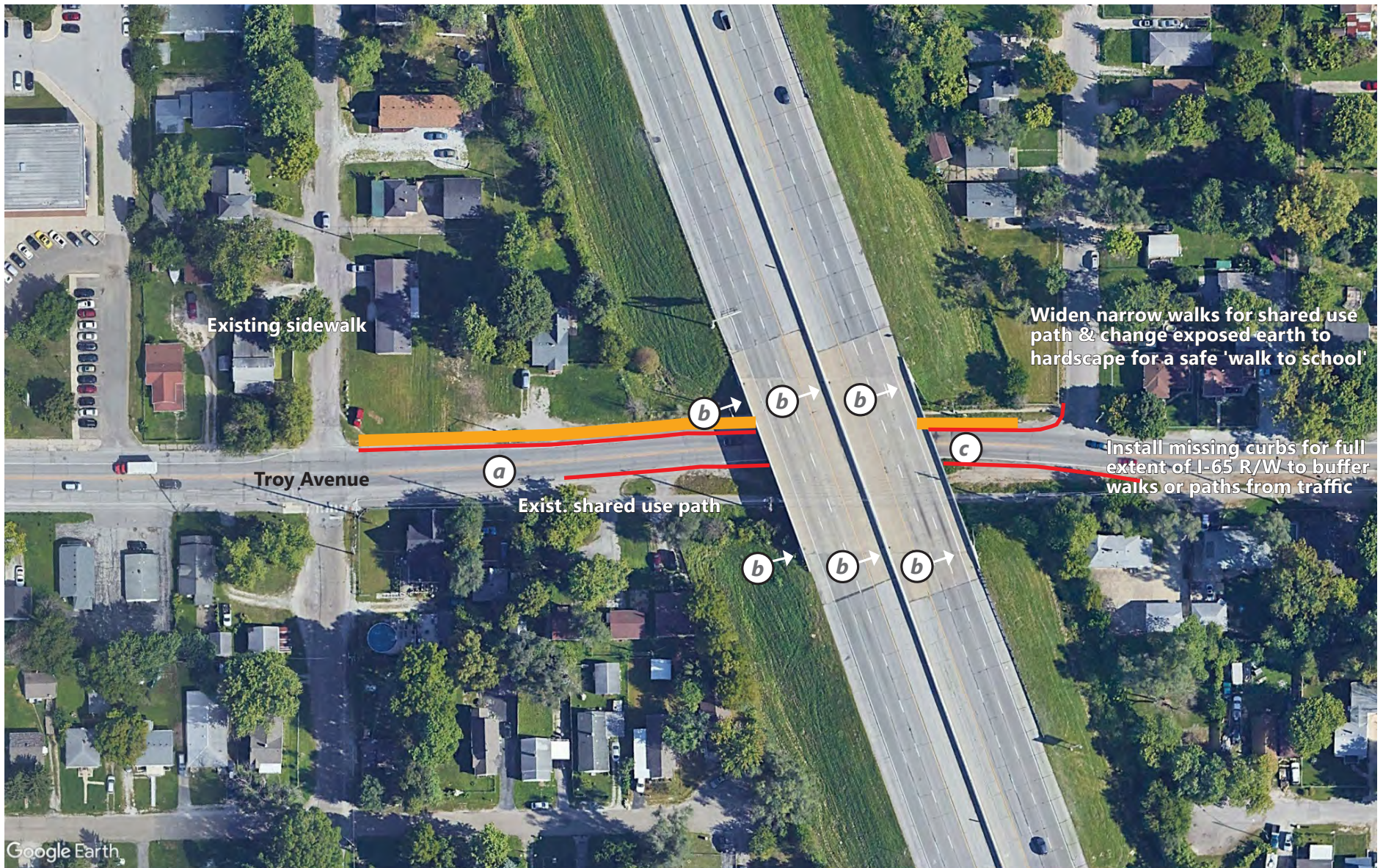
I-65 passes over Troy Ave, designated a Primary Arterial in the *Indianapolis Thoroughfare Plan*. Its four lanes west of the underpass transition to two lanes through and east of the underpass.

Although the SAFE project's proposed scope of work only includes I-65 resurfacing and re-striping at this location, the underpass conditions caused by the interstate require a broader consideration of that scope to resolve several environmental justice issues regarding its role as a portal for neighborhoods east of the interstate to both the Donnan Elementary/Middle School and to a Red Line BRT stop on Shelby Street.

The BRT connects UIndy to downtown and beyond.

The south half of the underpass is now occupied by a multi-use path impacted by bridge drainage and erosion. The north half has a narrow sidewalk mostly covered by roadway debris and eroded soils caused in part, if not entirely, by interstate drainage directed to the uncurbed and unvegetated shoulder areas of the underpass. While residents passionately conveyed to the Rethink Coalition team their daily need to pass through this portal, their hope for a safe passage is silently conveyed by a mural painted on the north slope wall, a testament to their need for a safe direct path between their homes, the school and public transit.

Study Area C: Troy Underpass Observations & Recommendations



AERIAL VIEW OF TROY AVE AT I-65

Study Area C: Troy Avenue Underpass Observations/Recommendations

Rethink Coalition Recommendations

- Address deficiencies of pedestrian and bicycle facilities through both sides of the underpass.
- Redirect bridge drainage away from the slopewalls.

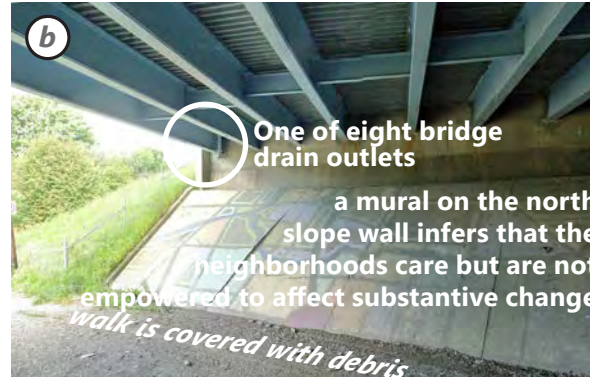
- Replace exposed bridge-shaded erodible soils with hardscape materials .
- Install curbs on all road edges within the I-65 right-of-way to guide vehicles and prevent their intrusion into walk areas.

- Install minimum eight-foot wide shared use path buffered from traffic through the underpass along the westbound lane..
- Work with the city to facilitate connectivity to and beyond the INDOT controlled interstate right-of-way area.



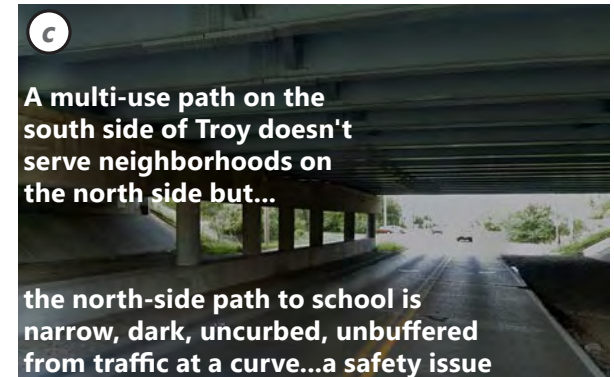
The SAFE project should address deficient underpass conditions such as this uncurbed narrow path to a school

The Troy Avenue bridge underpass is characteristic of many if not all such portals between neighborhoods separated by the Interstate. This instance is particularly egregious occurring along a pathway to an IPS elementary/middle school and a Red Line bus rapid transit stop. Although there is a shared use path along the eastbound travel lane there is no comparable passage on the westbound side leading to the school.



Four bridge inlets at each end of the spans drop water and debris on walkway areas

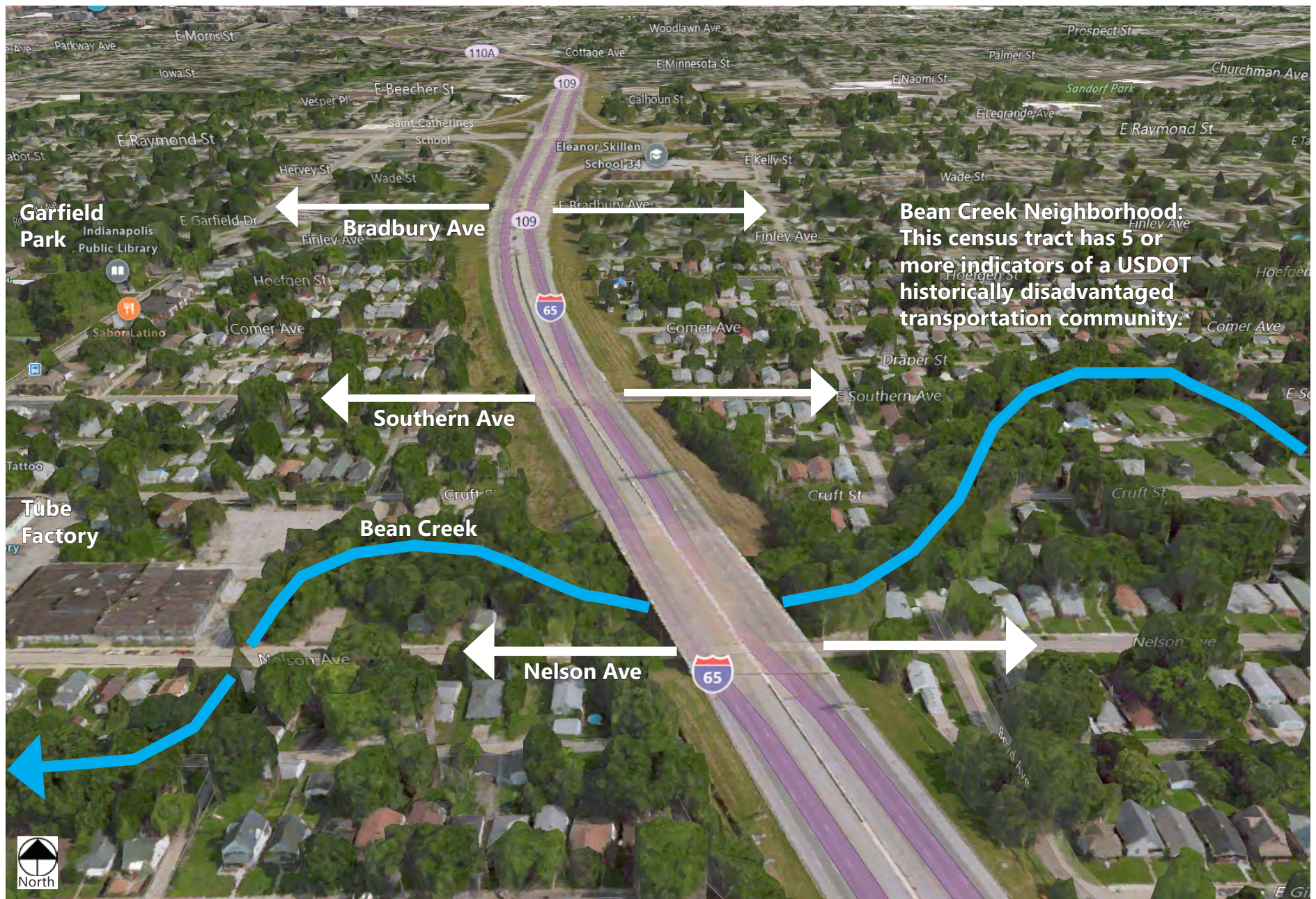
Bridge storm water drains to the underpass depositing highway pollutants and debris, causing bare earth erosion and icing of shaded walks that discourages elementary and middle school students from walking or cycling to school.



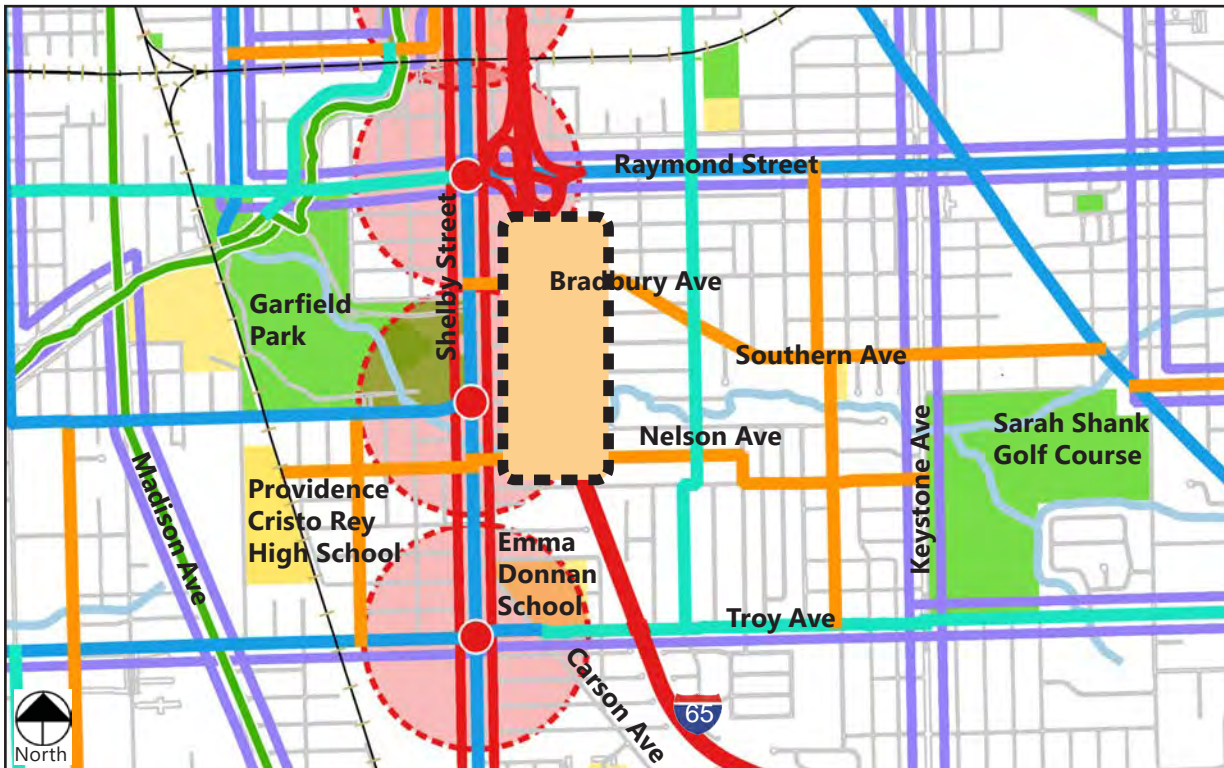
The dark uncurbed underpass along a curve in the road is a safety concern for pedestrians and bicyclists.

The absence of effective lighting of the dark side slope areas further diminishes the sense of safety and security for all users including those for whom walking or cycling to the Red Line stop may be a need rather than a choice.

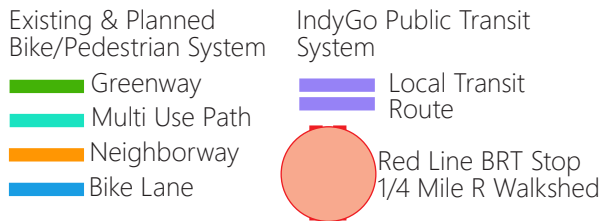
Study Area D: Nelson/Southern/Bradbury Portals



Study Area D: Nelson/Southern/Bradbury Portals



**NELSON/SOUTHERN/
BRADBURY
PORTAL STUDY AREA**



RETHINK COALITION OBSERVATIONS

Nelson, Southern and Bradbury are all designated Local Streets in the *Indianapolis Thoroughfare Plan*.

The *Indy Moves* plan designates Nelson and Bradbury Avenues as *neighborway* bicycle facilities, where most cyclists can safely use low volume, low speed streets through the underpass.

The proposed scope of work for the interstate in the I-65 SAFE Project is indicated as resurfacing and re-stripping. No work is indicated for the underpasses.

Recommendations:

- Fix scupper drainage
- Pave bare dirt both sides
- Improve lighting
- Relocate R/W fence to ease mowing and facilitate maintenance

Study Area D: Nelson/Southern/Bradbury Bridges



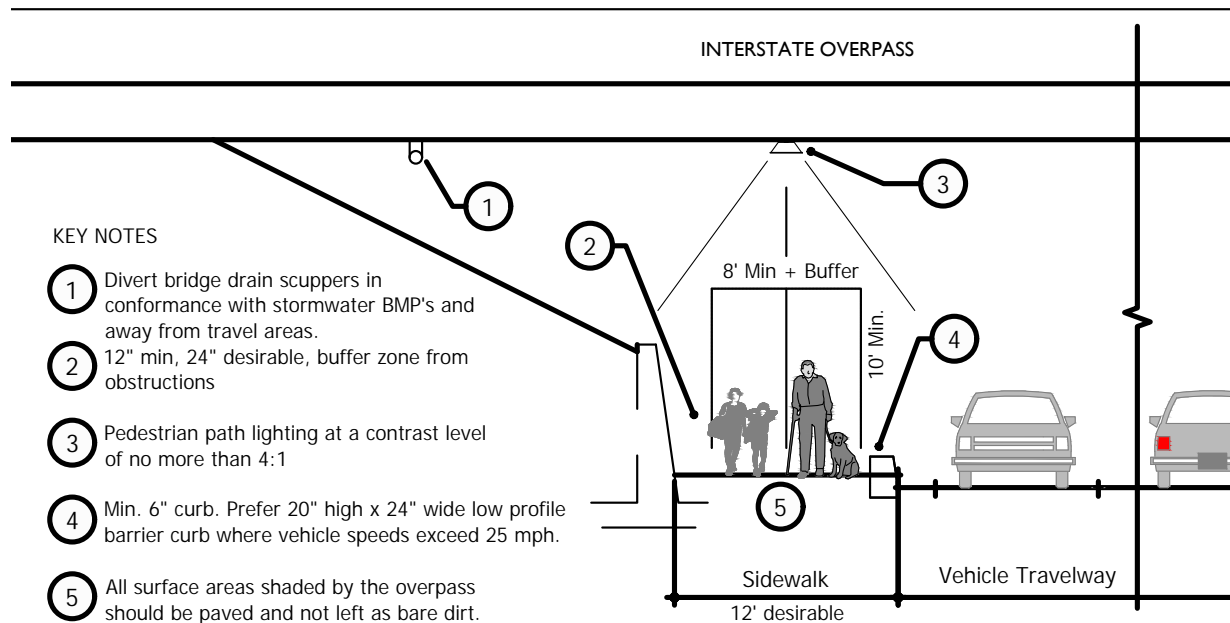
Nelson Avenue and Bean Creek Underpass



Southern Avenue Underpass



Bradbury Avenue Underpass



Comments applicable to these underpasses.

- Generally underpasses in good condition; though fairly long and dark.
- Recommendations: pave bare dirt, redirect drain scuppers from walkways and creek, improve lighting frequency and light slope wall recesses.
- Evaluate chain link fence locations to maintain highway safety but optimize for easier mowing.

Rethink Coalition Design Guideline for Bridge Underpasses

Study Area E: Raymond Street Interchange Context



Raymond Street is a nearly twelve-mile long divided primary arterial highway and Tier 3 freight corridor with four lanes west of I-65 to the Sam Jones/I-465 interchange and six lanes east of I-65 to the I-74/I-465 interchange [via Southeastern Ave]. The east leg performs as a virtual bypass extension of I-74 to northbound I-65 saving 2.5 travel miles but often burdening the Raymond Street interchange beyond its capacity. That traffic volume contributes to

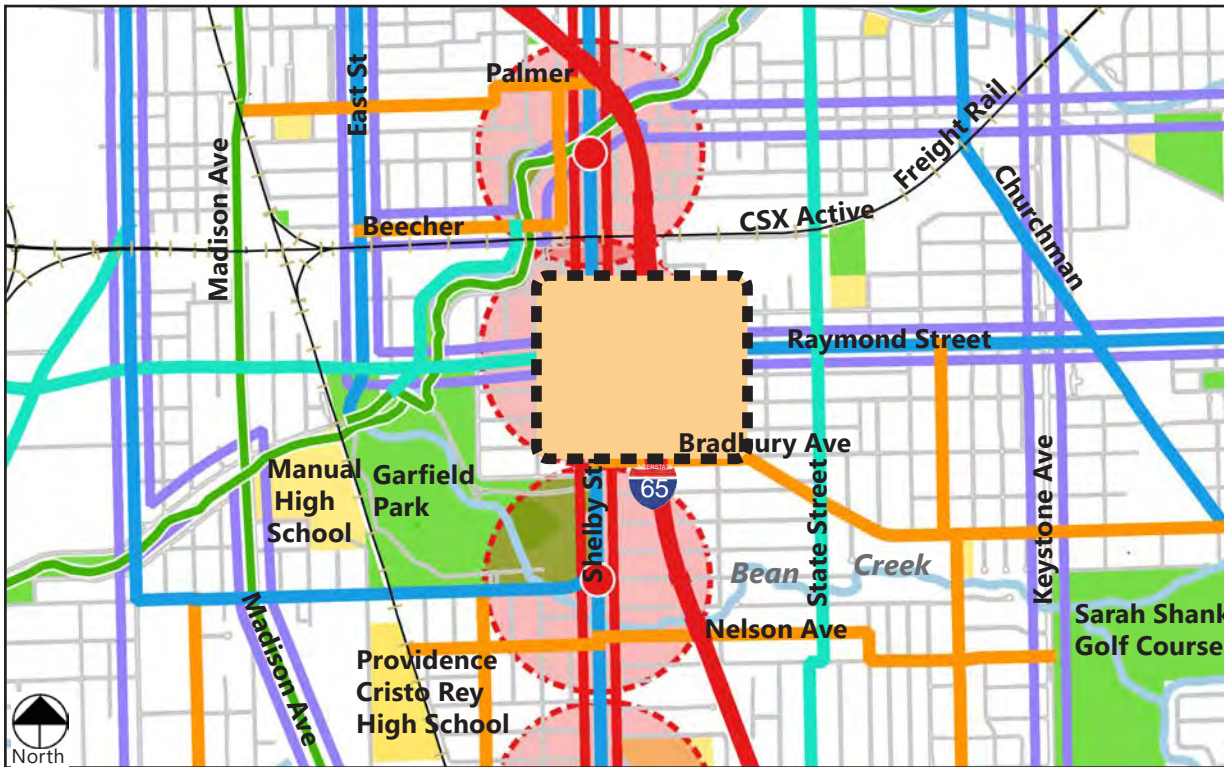
the identified freight traffic "bottleneck" that at times stretches from this interchange into and beyond the South Split of the 65/70 Inner Loop. Mitigating that bottleneck congestion is a purpose of the I-65 SAFE project.

But the Raymond Street interchange underpass is also a portal between neighborhoods east of I-65 and destinations to the west, many clustered around or near Shelby Street, including

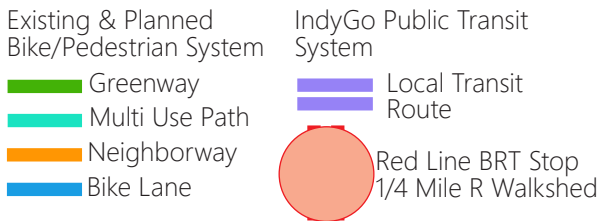
convenience shopping, employment venues and a Red Line bus rapid transit station to UIndy, Garfield Park, Fountain Square, Downtown, IU/Purdue, IU Health and north past Broad Ripple.

The I-65 Safe project excludes consideration of remedies to serious barriers to the portal's neighborhood connectivity function. This report proposes specific measures that address that omission as well as interchange congestion itself.

Study Area E: Raymond Street Interchange Context



RAYMOND STREET INTERCHANGE PORTAL STUDY AREA



Rethink Coalition Observations

This interchange exemplifies the need for the I-65 SAFE project to eliminate costly widening of three bridges and expansion of pavement beyond existing outer shoulders from the project's scope. Those costs are more appropriately applied to correcting an array of mobility and connectivity issues that make this and Study Area G particularly problematic for all users.

The multimodal connectivity diagram above, derived from the City and IndyGo, shows the complexity of those systems' interface with the interchange. Close inspection reveals multiple

conflict points between the interchange function and local connectivity to the point of avoidance by populations otherwise reliant on that connectivity. At the same time, the interchange function itself is hobbled by correctable geometric issues:

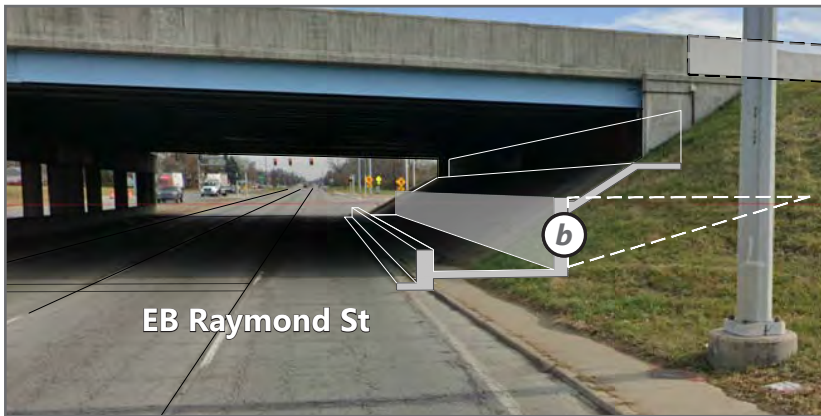
Interchange functional issues:

1. The partial cloverleaf interchange used throughout the rural stretches of I-65 does not fit this urban context.
2. Its 150-foot radius ascending access ramp to southbound I-65 limits acceleration while forcing its adjoining exit ramp too near an arterial intersection.
3. That signalized exit is located less than five car lengths [or 1.5 WB-67 tractor-trailer lengths] from signalized Raymond Street.
4. Result: Spillback blocking condition is frequent and signal progression through the interchange is impossible.

Pedestrian/Cyclist Issues:

1. The sidewalks and underpasses are generally not bicycle friendly, needing a minimum of eight-foot width or more where walks are adjacent to the curb lines to provide a buffer.
2. The on-ramps are a high-speed geometric that pose hazards to pedestrians and cyclists because cars are intended to yield and pedestrians have their back to traffic.
3. Efforts to correct that can have an impact on ramp capacity.

Area E: Raymond Street Interchange Underpass



Creating a safe portal for cyclists and pedestrians

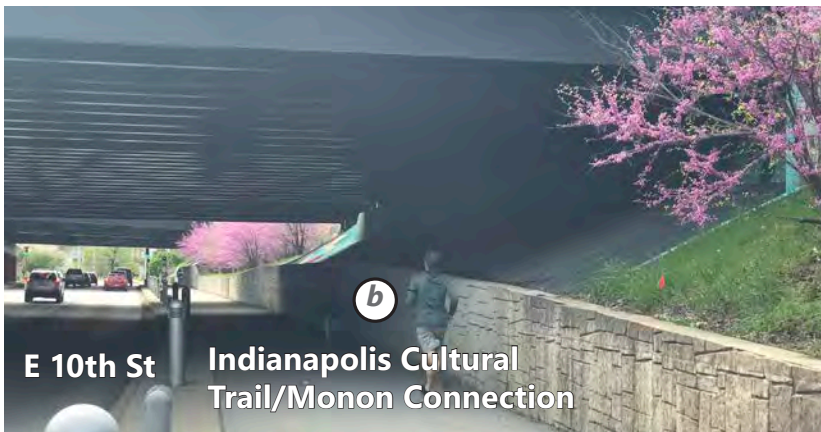
- 1 Truncate slopewall, add retaining kneewall to accommodate safe pedestrian/bicycle passage through underpass.
- 3 Maintain constant curb offset by removing existing curb/walk flare towards travel lane.
- 4 Bend pathway behind sign pole to maintain required pathway buffer from traffic & objects.
- 5 Supplement existing roadway lighting for balanced day/night luminance on pathway & slopewall surfaces.



Precedence for constructability and performance

An identical condition existed at the E 10th Street underpass of the I-65/70 North Split bridges in 2003. The sidewalk was the connection between the Monon Trail south terminus and the north terminus of the Indianapolis Cultural Trail.

The E 10th Street underpass was avoided by pedestrians and cyclists because of safety, poor lighting, and fast traffic close to the unprotected curb edge. That contributed to a perception of danger that resulted in avoidance. This condition impacted both east/west and north/south connectivity.



A celebrated and transformational passage was achieved by the INDOT 2020 Hyperfix Project

That project, similar in scale to the I-65 SAFE Project, developed improvements to the E 10th Street underpass that transformed the connection between Mass Ave's commercial district and supercharged the economic development of the E 10th Street area.

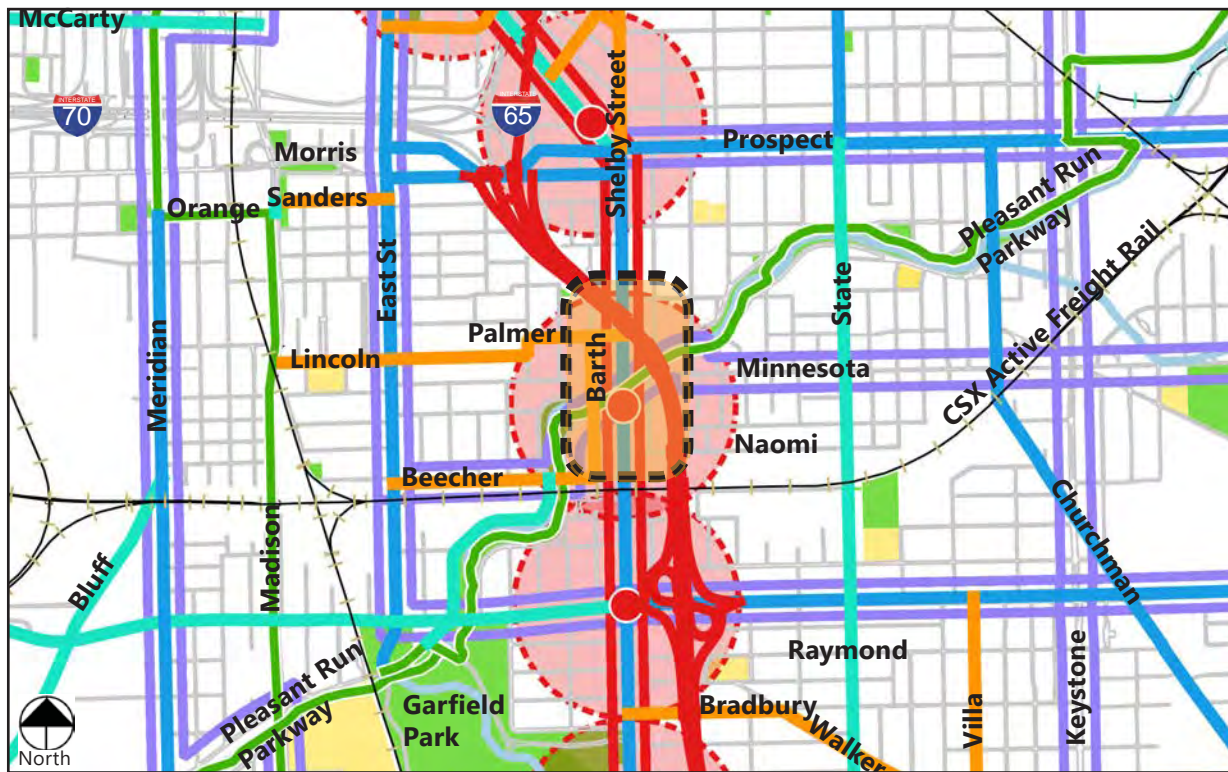
Later enhanced by the Superbowl initiative, Hyperfix constructed 600 feet of retaining wall. The Raymond Street interchange underpass would need approx. 300 feet.

Area F: CSX/Naomi/Pleasant Run & Shelby Street Bridges Context



I-65 bridges are "Neighborhood Portals" over Naomi, Pleasant Run N&S, and Shelby.

Area F: CSX/Naomi/Pleasant Run & Shelby Street Bridges Context



NAOMI STREET, PLEASANT RUN & SHELBY STREET PORTALS STUDY AREA

- Existing & Planned Bike/Pedestrian System
 - Greenway
 - Multi Use Path
 - Neighborway
 - Bike Lane
- IndyGo Public Transit System
 - Local Transit Route
 - Red Line BRT Stop
 - 1/4 Mile R Walkshed

Pleasant Run Parkway Impacts & Remediation Recommendation

The parkway is a component of the *Indianapolis Historic Park and Boulevard System* listed on the National Register of Historic Places. Rethink recommends that the SAFE I-65 project remediate existing drainage/erosion and water quality impacts contributed by the interstate overpass to create a NEPA Section 4F *Net Benefit* to the resource effected by project implementation. Rethink recommendations address how to achieve net benefit.

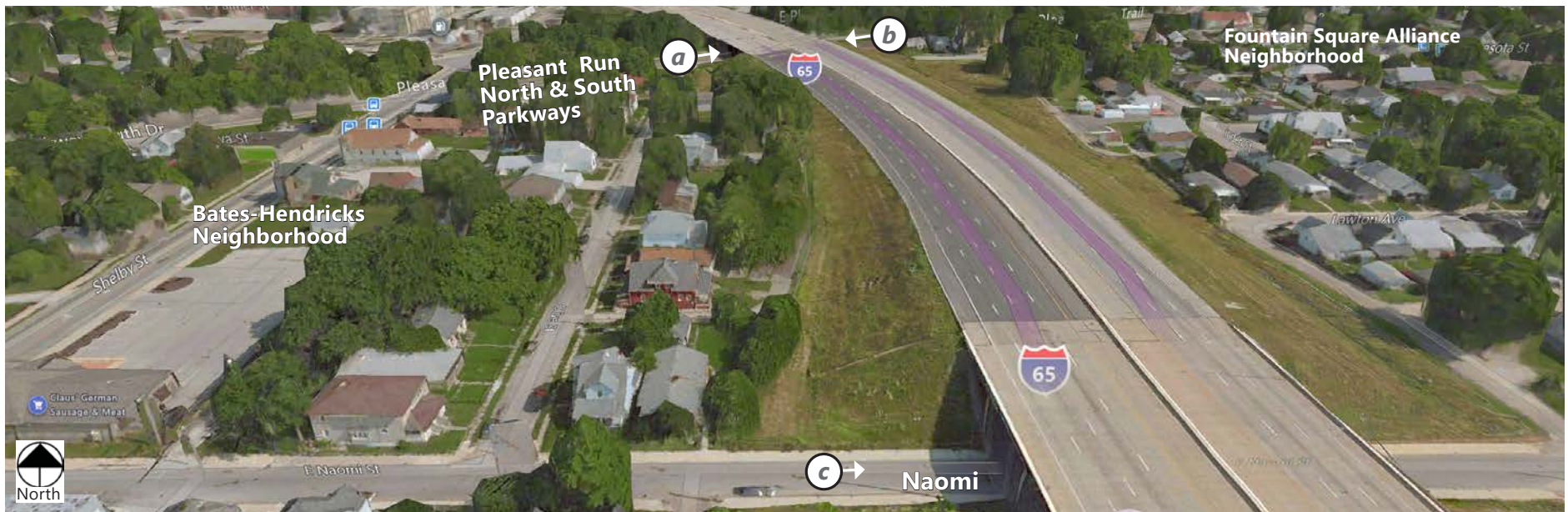
RETHINK COALITION OBSERVATIONS

Naomi and Pleasant Run North and South Parkways are designated Local Streets although a section of the latter is both an IndyGo route and a Tier 4 Freight Corridor through the parkway's I-65 underpass. A major Greenway aligns with the south Pleasant Run Parkway.

Primary Arterial designated Shelby Street is both a local and BRT route, a Tier 4 Freight Corridor, and an important two-way protected bike lane. Both underpass corridors have multiple but correctable deficiencies. The Shelby bridge skew exasperates the narrow right-of-way in accommodating the corridor's multimodal functions. Most deficiencies can be remediated as part of the I-65 SAFE project in partnership with the city as a joint undertaking.

Naomi Street is a local street with much less traffic. It is also an important neighborhood portal to Shelby Street transit and bicycle facilities as well as to a Neighborway link to the Pleasant Run Greenway and Garfield Park. Walking or cycling through the underpass to reach those destinations is disincentivised by the underpass's correctable deficiencies. Those include interstate bridge drain outlets that direct stormwater and roadway debris directly onto deteriorating slope walls and across sidewalks wedged between the slopewalls and curbs, unswaled turf embankments that sheet flow erosion onto the already narrow walks and the adjacent pavement, and awkwardly located, poorly maintained right-of-way fencing.

Area F: Naomi & Pleasant Run Observations



Pleasant Run Parkway South [View east]
The greenway path connecting multiple neighborhoods is impacted by roadway debris and eroded exposed soils alongside an uncurbed freight and transit-serving roadway. The apparent brightness is a camera-created illusion.



Pleasant Run Parkway North [View west]
There are no sidewalks or protective curbs to serve Fountain Square Neighborhood residents walking or cycling to nearby Shelby Street transit or bike facilities seen beyond, though space exists for both. Drainage issue is apparent.



Naomi underpass [View east]
Both north and south slopewall toes limit useful sidewalk width, while rubble & fencing intrudes further. The bridge drains stormwater and interstate debris towards erosive surfaces, walks and the street [not addressed by the I-65 SAFE project].

Area F: Shelby Street Underpass Observations of Existing Conditions



Shelby Street [View south]

Very narrow southbound travel lanes on a freight and transit corridor threaten counterflow cyclists protected only by flexible wands and a low curb. Excess northbound lanes coexist with a too narrow ADA-noncompliant sidewalk.



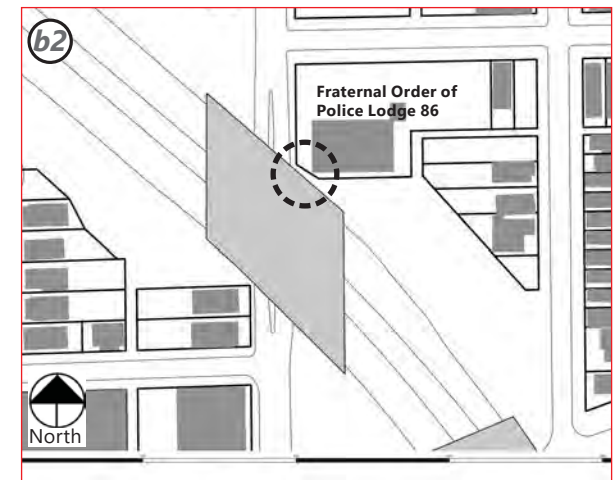
Shelby Street [View north]

In spite of excess pavement and sidewalk width under the east side of the bridge, the sidewalk width beyond has a pedestrian pinch point between a utility pole and fence that is non-compliant with ADA requirements.



Traffic Conflicts - N side of Shelby Bridge

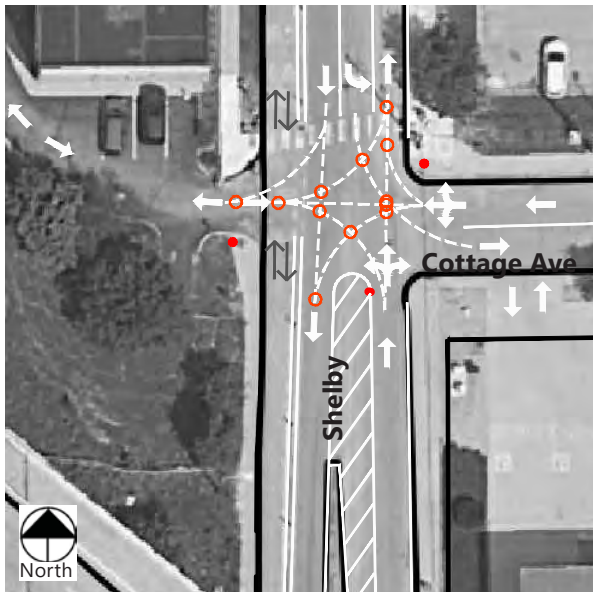
Multiple turning movements from Cottage Ave and the Eskenazi clinic in combination with the Cycle Track, create at least eleven traffic conflicts, posing risks to all users, esp. pedestrians.



Map Indy Parcel Lines

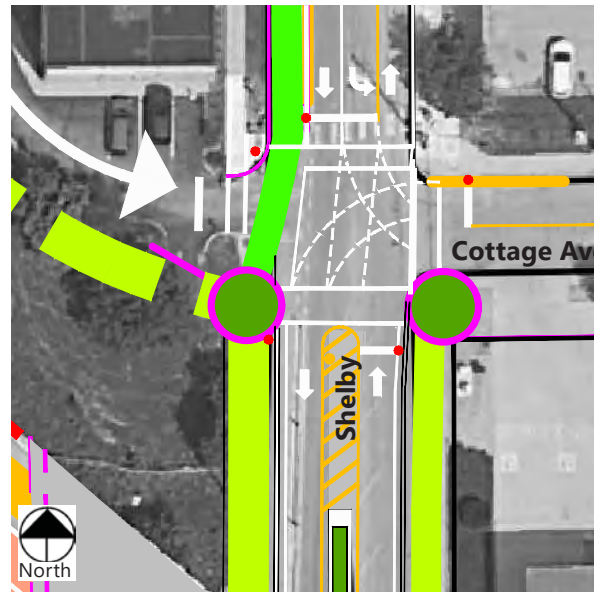
The property line is chamfered at Fraternal Order of Police property. Fence should be aligned with parcel line to allow safe pedestrian travel.

Area F: Shelby Street Underpass Observations



Traffic Conflicts - N side of Shelby Bridge

Multiple turning movements from Cottage Ave and the Eskenazi clinic in combination with the Cycle Track, create at least eleven traffic conflicts, posing risks to all users, esp. pedestrians.



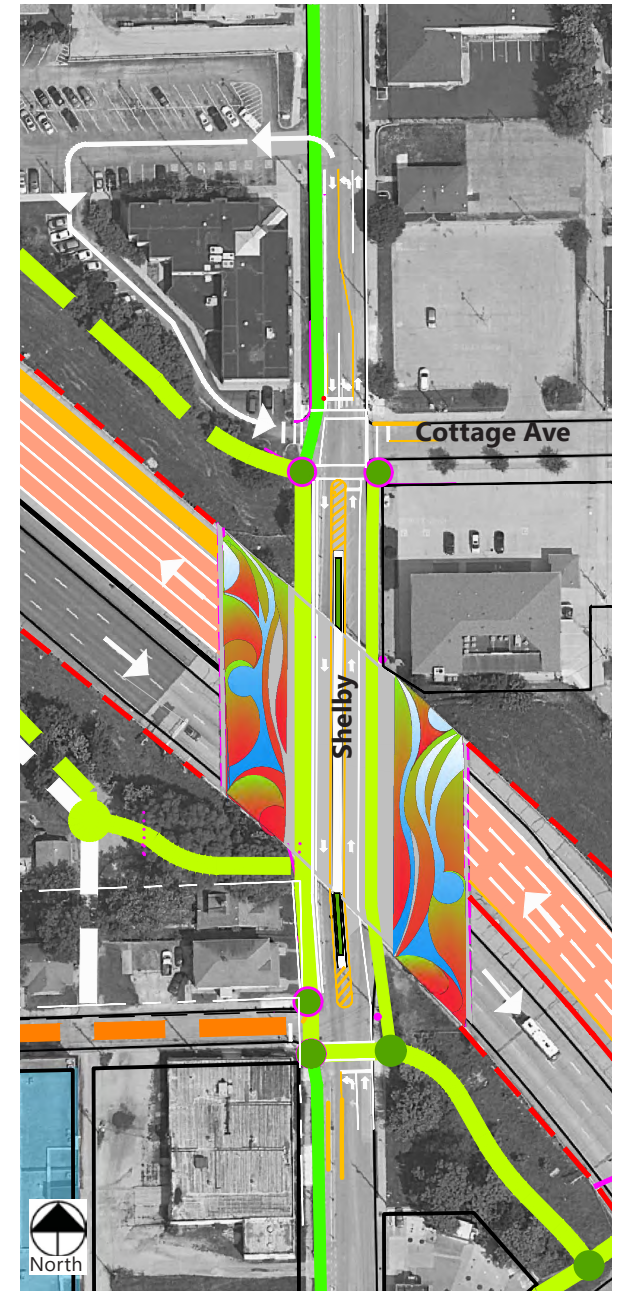
Opportunity (above)

Establish partnership with Eskenazi to modify parking lot movements across from Cottage Ave to be one way.

Install pedestrian crosswalk markings and stop bars as shown that connect the Cycle Track with the Interstate and Pleasant Run Greenway.

Opportunity (to the right)

Consider partnering with the Bates-Hendricks and Fountain Square Alliance neighborhoods for an inspiring mural on underpass side slopes.



Area F: Naomi/Pleasant Run & Shelby Street Recommendations

Rethink Recommendation

Create a Bates-Hendricks Fountain Square Alliance and Garfield Park Greenway Loop

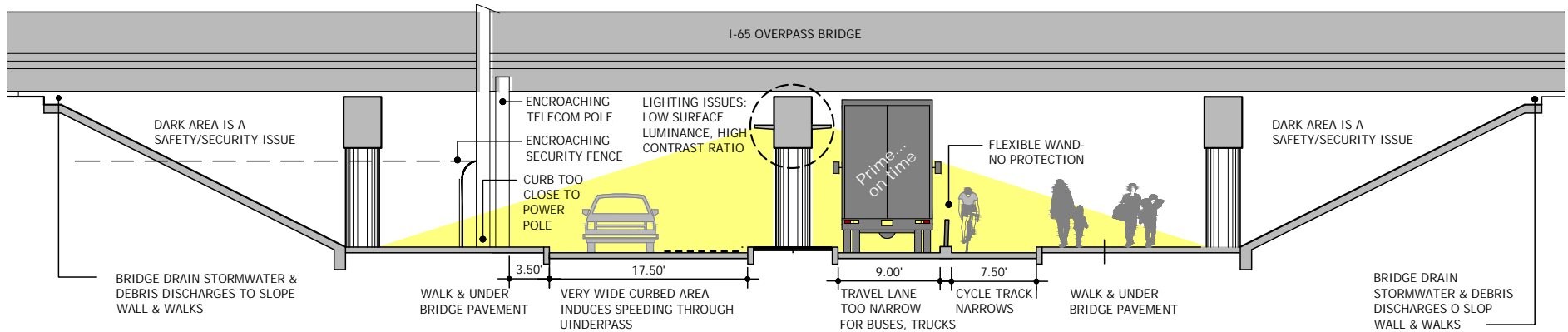
Per Rethink recommendations for the *Morris/Prospect Street Interchange and Bridges*, to focus on inner lane shoulder accommodation of the four-lane expansion leading into the South Split rather than outer lane expansion to accommodate an auxiliary lane, means the current bottleneck at the 65/70 split can be managed without bridge expansion at the Naomi and the Pleasant Run bridges.

Shown in the sketch are possible greenway or multiuse path connections that will break down the barrier of the Interstate by utilizing both City and INDOT right-of-way to provide critical links between Bates-Hendricks, Fountain Square Alliance and Garfield Park neighborhoods as a Greenway Loop.

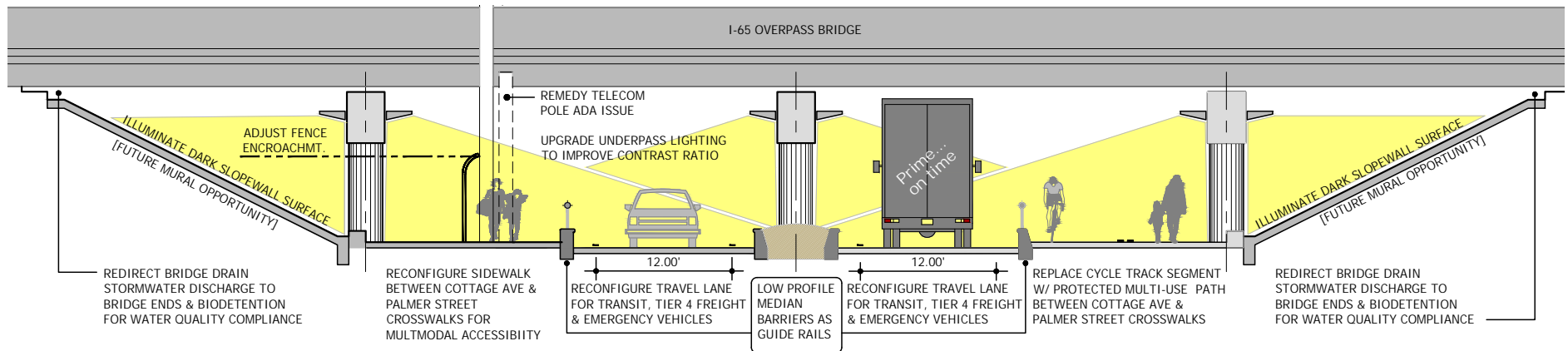


I-65 Bridges are "Neighborhood Portals" over Naomi, Pleasant Run and Shelby Streets

Area F: Shelby Street Underpass Recommendations



Existing Shelby Street Underpass Looking South



Recommended Shelby Street Underpass Looking South

Heard: "Shelby Street underpass is a place to be avoided".

Very narrow vehicle lanes along a truck and transit corridor, tight against a narrowed cycle track protected by delineators, and AM/PM dark.

Recommendations:

- Reconfigure all lanes to minimum standards.
- Reconfigure intersections at each end to reduce conflicts, and convert cycle-track to shared-use path.
- Protect bridge columns and bike/ped lanes & walks with low-profile median barriers.

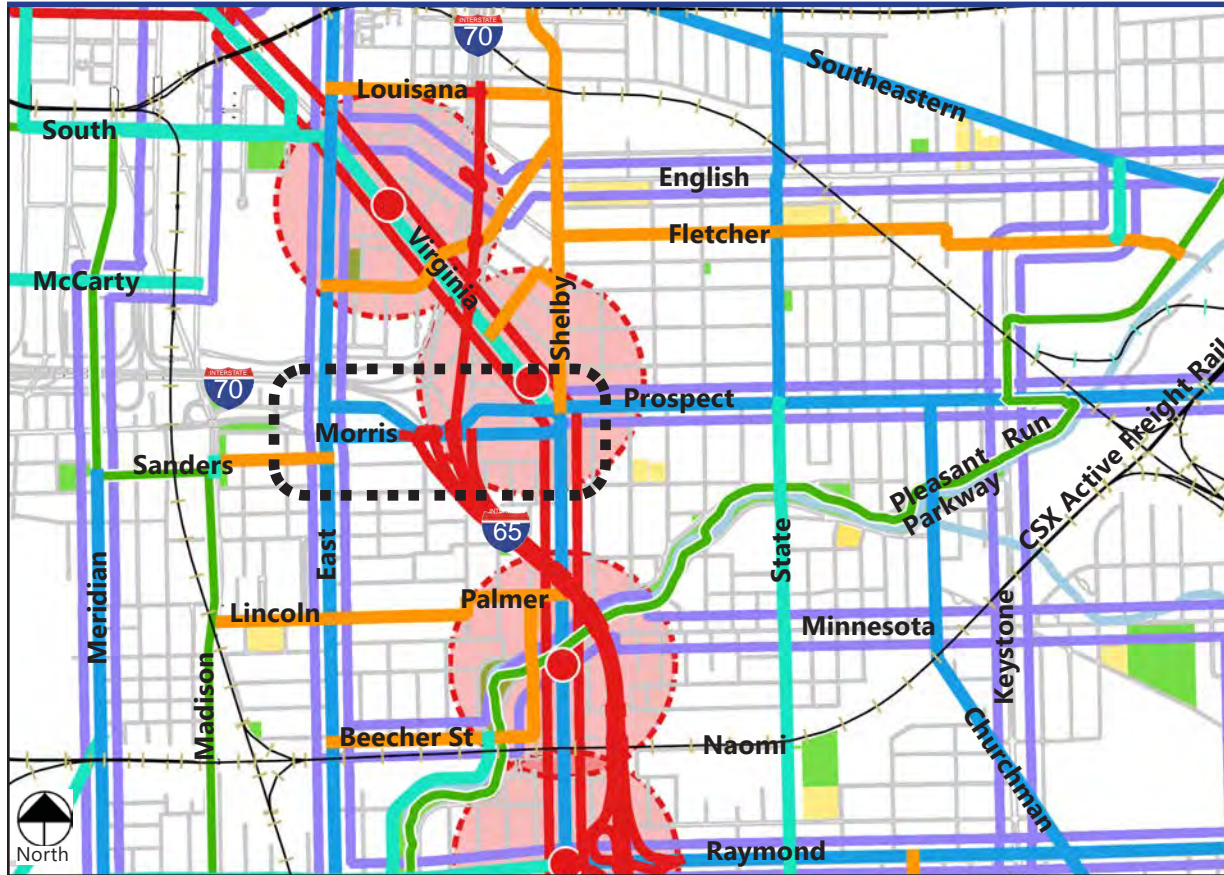
- Design/install a balanced day/night lighting system.
- "Permit" a well-lit mural to Celebrate Shelby Street (Per the 10th St Connec10n).

Area G: Morris/Prospect Street Interchange and Bridges



*View North along I-65 towards the Morris Street/Prospect Street Interchange and the I-65/I-70 South Split beyond.
June 5, 2023, A-V-A Video Productions*

Area G: Morris/Prospect Street Interchange and Bridges Context



LOCATION & CONTEXT OF MORRIS/PROSPECT STREET INTERCHANGE & BRIDGES

RETHINK OBSERVATIONS

The Morris/Prospect Interchange area is a high volume access point to and from I-65 for the southeast sector of downtown Indianapolis. That sector includes the Gainbridge Fieldhouse with 19,000 concert seats, Eli Lilly Corporate Headquarters with 7000 employees and other major employers and destinations. The interchange is unique as the only interstate interchange serving downtown Indianapolis from outside the Inner Loop. The convergence of the Morris/Prospect one-way pair's crosstown traffic with downtown commuter traffic compounds the interstate-caused separation of the Bates-Hendricks and Fountain Square neighborhoods.

In traffic engineering terms, this interchange has a volume/capacity ratio issue in which local street characteristics [capacity] cannot accommodate interchange demand without significant impacts on neighborhood connectivity and quality of life.

The I-65 Safe project proposes to widen the I-65 NB bridge over Morris/Prospect to add an "auxiliary" lane from Raymond to the Washington Street exit. Rethink Coalition opposes widening all bridges, and in particular this bridge, to allow the best long-term solution to be determined through the Rethink Coalition Reconnecting Communities Planning Grant and ProPEL Indy work. Rethink Coalition urges INDOT to allow these planning studies to have a full range of options to explore, including depressing the I-65 NB lane under Morris/Prospect Street. (continued)

A problematic Interchange

The Morris/Prospect one-way pair of streets is classified as a *Primary Arterial* in the *Indianapolis Thoroughfare Plan*. That designation's geometric standards, partially applied to these streets, conflict with the land use characteristics and connectivity needs of the Bates-Hendricks and Fountain Square neighborhoods separated by I-65.

NAOMI STREET, PLEASANT RUN & SHELBY STREET PORTALS STUDY AREA

- Existing & Planned Bike/Pedestrian System
 - Greenway
 - Shared Use
 - Neighborway
 - Bike Lane
- IndyGo Public Transit System
 - Local Transit Route
 - Red Line BRT Stop 1/4 Mile Walkshed

Area G: Morris/Prospect Street Interchange and Bridges

Instead Rethink Coalition urges INDOT to address significant interchange issues and opportunities that impact the City's ability to accommodate local transportation needs, including freight, bicycle/pedestrian connectivity and improve safety for all users that serve or co-locate with the interchange. Recent crosswalk improvements do not address bicycle usage and remain problematic for pedestrians in the absence of any change to the high-speed geometric design of the interchange elements. The Morris/Prospect Interchange area is unaccommodating of pedestrian/cyclist connectivity.

Correcting that is not *currently* part of the Safe project scope which is focused on mainline improvements. On-street parking is partially time-restricted to accommodate PM rush hour traffic out of the downtown generated by the Lilly campus, which is both an enforcement problem and a neighborhood inconvenience.

Morris-Prospect East of the Interstate

The two streets constitute a two-lane primary arterial separated one block as a one-way pair, each with a single travel lane and unrestricted parking on one or both curb lines. Both experience congestion of a level to be expected given the development mix and density, and the interruption of many east-west local streets by the interstate. While currently categorized as Primary Arterial, that classification needs to be reconsidered once the future RCPP and the larger ProPEL study are completed.

Prospect is one westbound travel lane between Virginia Ave and Leonard Street with unrestricted parking along both curbs.

Morris is one eastbound travel lane and one unrestricted parking lane between Leonard and St. Patrick Streets, widening to 3-lane with no parking to Shelby Street.

Morris Street west of I-65

Morris is constrained by its residential development pattern to two eastbound lanes from East Street to the SB I-65 ramp, one of which is time restricted parking. It continues over the interstate as two lanes described above even though it narrows down to a single lane beyond.

Prospect West of I-65

Morris-Prospect over I-65/I-70 and under I -70 WB ramp and I-65 North [part of the South Split].

Morris-Prospect join each other here, separated by a median, to constitute a 700-foot long divided primary arterial between the NB and SB I-65 ramps, half of which is an interstate overpass with an additional WB left turn lane onto SB I-65.

The bridge deck is 66-foot wide between curbs, divided in two longitudinal sections of 37-foot wide WB and 24' wide EB between curbs. The sidewalks abutting the curbs are less than the minimum six-foot width for that condition and disincentivises pedestrian use, while the roadway configuration does likewise for bicycle

use, thus denying the sizeable Bates-Hendricks neighborhood safe, convenient multi-modal connectivity to its major destination, the Fountain-Fletcher Virginia Avenue corridor.

Recommendations

Rethink Coalition supports INDOT's proposal to add a lane from I-465 to I-70 for a total of four lanes in each direction to reduce congestion and improve safety.

Rethink Coalition urges INDOT to consider fiscal prudence and defer any bridge widening due to the Reconnecting Communities grant and ProPEL Indy Study.

Rethink Coalition suggests that additional Washington Street exit capacity can be achieved by widening north of the Morris/Prospect I65 NB Bridge as a short-term solution to meet the project's purpose and need without widening bridges.

Rethink-proposed countermeasures to local transportation congestion issues in this area range from modest near-term to major longer-term actions.

This report identifies median barrier improvements that will correct current bottleneck pinch-points without bridge expansion, while also potentially eliminating the need for sound walls through enhanced mitigation of wheel noise.

Area G: Morris/Prospect Street Interchange and Bridges



**MORRIS/PROSPECT RECONFIGURATION
BIKE/PED CONNECTIVITY BETWEEN BATES-HENDRICKS & FOUNTAIN SQUARE**

Morris/Prospect Interchange & Portal

Morris-Prospect is a nearly 700-foot gap between Bates-Hendricks and Fountain Square Neighborhoods. It has the potential to be a powerful multimodal connector.

Recommendations:

a

Reconfigure four travel and one turn lane to a lengthened turn lane and one travel lane in each direction. Create bike/ped shared use paths in place of existing narrow walks and outer travel lanes.

b

Change ramp entrances from free-flow to all-way stop for bike/ped safety & metered gap flow to I-65. All way stops are an effective alternative to delay-inducing signals and are feasible for single-lane configuration for ramp entrances and Leonard north of Morris Street.

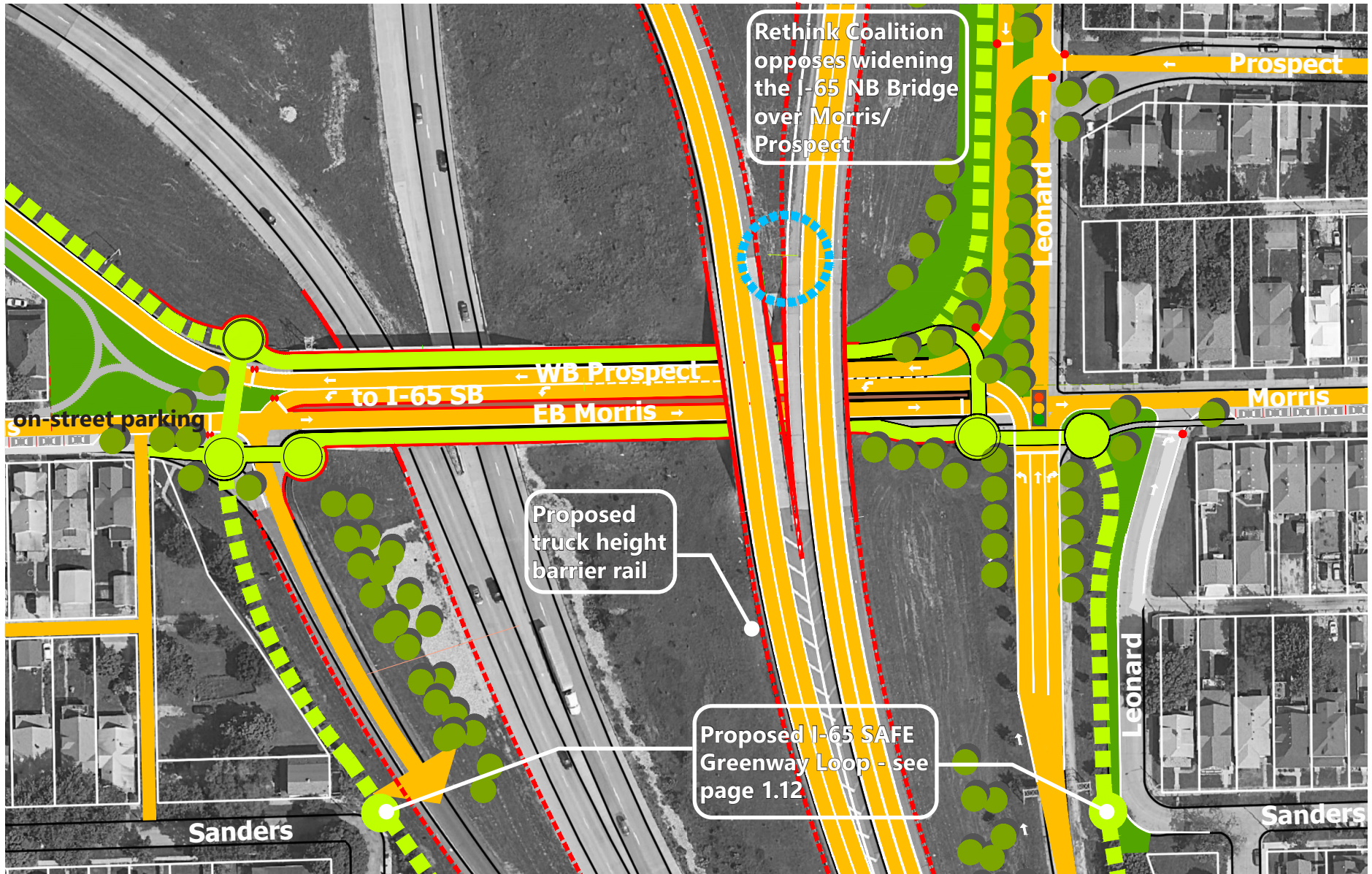
c

Eliminate parking restrictions for one travel lane on eastbound Morris from East Street with Street tree curb extensions.

d

Two pinch points constitute the travel bottleneck for the Raymond to Prospect section of northbound I-65. Modify the transition between the bridge rails and the w-beam rails that are now skewed inward to create a continuous width median shoulder sufficient to carry the four-lanes to split over Morris/Prospect.

Area G: Morris/Prospect Street Interchange and Bridges



Morris Prospect Lane Re-Configuration to accommodate Bates-Hendricks/Fountain Square Connectivity

Area G: Morris/Prospect Street Interchange and Bridges

I-70 WEST & I-65 NORTH BRIDGES BEYOND

